

SCHEDULE 4

Regulation 5(2)(b) and (3)(b) and 6(2) and  
(3)

**PART I**

**ROAD MARKINGS TO INDICATE PELICAN AND PUFFIN CROSSINGS,  
PELICAN AND PUFFIN CONTROLLED AREAS AND STOP LINES**

**Interpretation of Schedule**

1. In this Schedule, except where otherwise stated,—
  - (a) a reference to a “crossing” is to a Pelican crossing or a Puffin crossing;
  - (b) a reference to a “controlled area” is to a Pelican controlled area or a Puffin controlled area;
  - (c) a reference to a numbered diagram is a reference to the diagram in Part II of this Schedule so numbered.

**Indication of limits of crossings and of controlled areas and stop lines**

2.—(1) Subject to the provisions of this Schedule, the limits of a crossing on a two-way street and of its controlled areas and stop lines shall be indicated by road markings consisting of lines and studs on the carriageway of the size and type shown—

- (a) in diagram 1 where there is no central reservation;
- (b) in diagram 2 where there is a central reservation, but the crossing does not form part of a system of staggered crossings; and
- (c) in diagram 3 where the crossing forms part of a system of staggered crossings.

(2) Subject to the provisions of this Schedule, the limits of a crossing on a one-way street and of its controlled areas and stop lines shall be indicated by road markings consisting of lines and studs placed on the carriageway of the size and type shown—

- (a) in diagram 4 where there is no central reservation;
- (b) in diagram 5 where there is a central reservation but the crossing does not form part of a system of staggered crossings; and
- (c) in diagram 6 where the crossing forms part of a system of staggered crossings.

(3) The two lines of studs indicating the limits of a crossing need not be at right angles to the edge of the carriageway, but shall form straight lines and shall, as near as is reasonably practicable, be parallel to each other.

**Controlled areas and stop lines on a two-way street**

3.—(1) Where a crossing is on a two-way street the road markings to indicate each controlled area and stop line shall consist of—

- (a) a stop line parallel to the nearer row of studs indicating the limits of the crossing and extending, in the manner indicated in the appropriate diagram, across the part of the carriageway used by vehicles approaching the crossing from the side on which the stop line is placed; and
- (b) two or more longitudinal zig-zag lines or, in the case of a road having more than one carriageway, two or more such lines on each carriageway, each zig-zag line containing not less than 8 nor more than 18 marks and extending away from the crossing.

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(2) Subject to paragraph (3), where a central reservation is provided, the road marking shown in diagram 1040 in Schedule 6 to the 1994 Regulations may be placed between the zig-zag lines on the approaches to the central reservation.

(3) Where a central reservation is provided connecting crossings which form part of a system of staggered crossings, the road marking shown in diagram 1040.2 in Schedule 6 to the 1994 Regulations shall be placed in the manner indicated in diagram 3.

(4) The distance between the studs and the terminal marks on the exit sides shall be not less than 1700 mm nor more than 3000 mm.

#### **Controlled areas and stop line on a one-way street**

4.—(1) Where a crossing is on a one-way street the road markings to indicate a controlled area and stop line shall consist of—

- (a) a stop line parallel to the nearer row of studs indicating the limits of the crossing and extending—
  - (i) in the case of a crossing of the type shown in diagram 4 or 5, from one edge of the carriageway to the other; and
  - (ii) in the case of a crossing of the type shown in diagram 6, from the edge of the carriageway to the central reservation; and
- (b) two or more zig-zag lines, each containing not less than 8 nor more than 18 marks and extending away from the crossing.

(2) Subject to paragraph (3), where a central reservation is provided, the road marking shown in diagram 1041 in Schedule 6 to the 1994 Regulations may be placed between the zig-zag lines on the approaches to the central reservation.

(3) Where a central reservation is provided connecting crossings which form part of a system of staggered crossings, the road marking mentioned in paragraph (2) shall be placed in the manner indicated in diagram 6.

#### **Variations in relation to a controlled area or stop line**

5.—(1) Where the traffic authority is satisfied that, by reason of the layout or character of the roads in the vicinity of a crossing, it is impracticable to indicate a controlled area in accordance with the requirements of the preceding provisions of this Schedule, the following variations shall be permitted—

- (a) the number of marks in each zig-zag line may be reduced to not less than 2;
- (b) the marks comprised in a zig-zag line may be varied to a length of not less than 1 metre, in which case—
  - (i) each mark in each zig-zag line must be of the same or substantially the same length as the other marks in the same line;
  - (ii) and the number of marks in each line must be not more than 8 nor less than 2.

(2) The angle of a stop line in relation to the nearer line of studs indicating the limits of a crossing may be varied, if the traffic authority is satisfied that the variation is necessary having regard to the angle of the crossing in relation to the edge of the carriageway.

(3) The maximum distance of 3 metres between a stop line and the nearer line of studs indicating the limits of the crossing shown in the diagrams in this Schedule may be increased to such greater distance, not exceeding 10 metres, as the traffic authority may decide.

(4) Each zig-zag line in a controlled area need not contain the same number of marks as the others and the pattern of the central lines may be reversed or, if the carriageway is not more than

6 metres wide, may be omitted altogether if replaced by the road marking shown in diagram 1004 in Schedule 6 to the 1994 Regulations.

### **Colour and illumination of road markings**

6. Subject to paragraph 7, the road markings shown in the diagrams in this Schedule shall be coloured white and may be illuminated by retroreflecting material.

### **Form and colour of studs**

7.—(1) The studs shown in the diagrams in this Schedule shall be—

- (a) coloured white, silver or light grey and shall not be fitted with reflective lenses; and
- (b) either circular in shape with a diameter of not less than 95 mm nor more than 110 mm or square in shape with the length of each side being not less than 95 mm nor more than 110 mm.

(2) Any stud which is fixed or embedded in the carriageway shall not project more than 20 mm above the carriageway at its highest point nor more than 6 mm at its edges.

### **Supplementary**

8. The requirements of this Schedule shall be regarded as having been complied with in the case of any crossing or controlled area, if most of the road markings comply with those requirements, even though some of the studs or lines do not so comply by reason of discolouration, temporary removal or a displacement or for some other reason, so long as the general appearance of the road markings as a whole is not thereby materially impaired.

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## PART II DIAGRAM 1

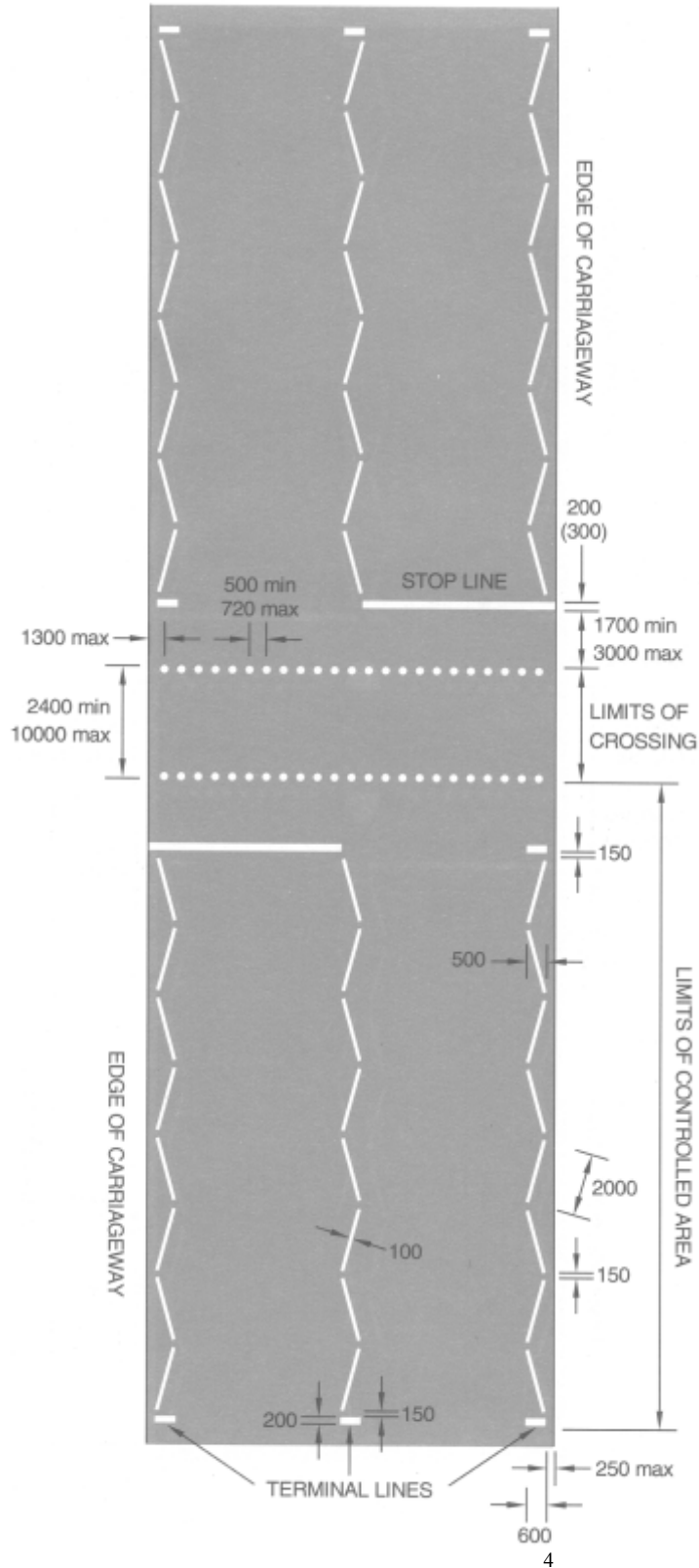
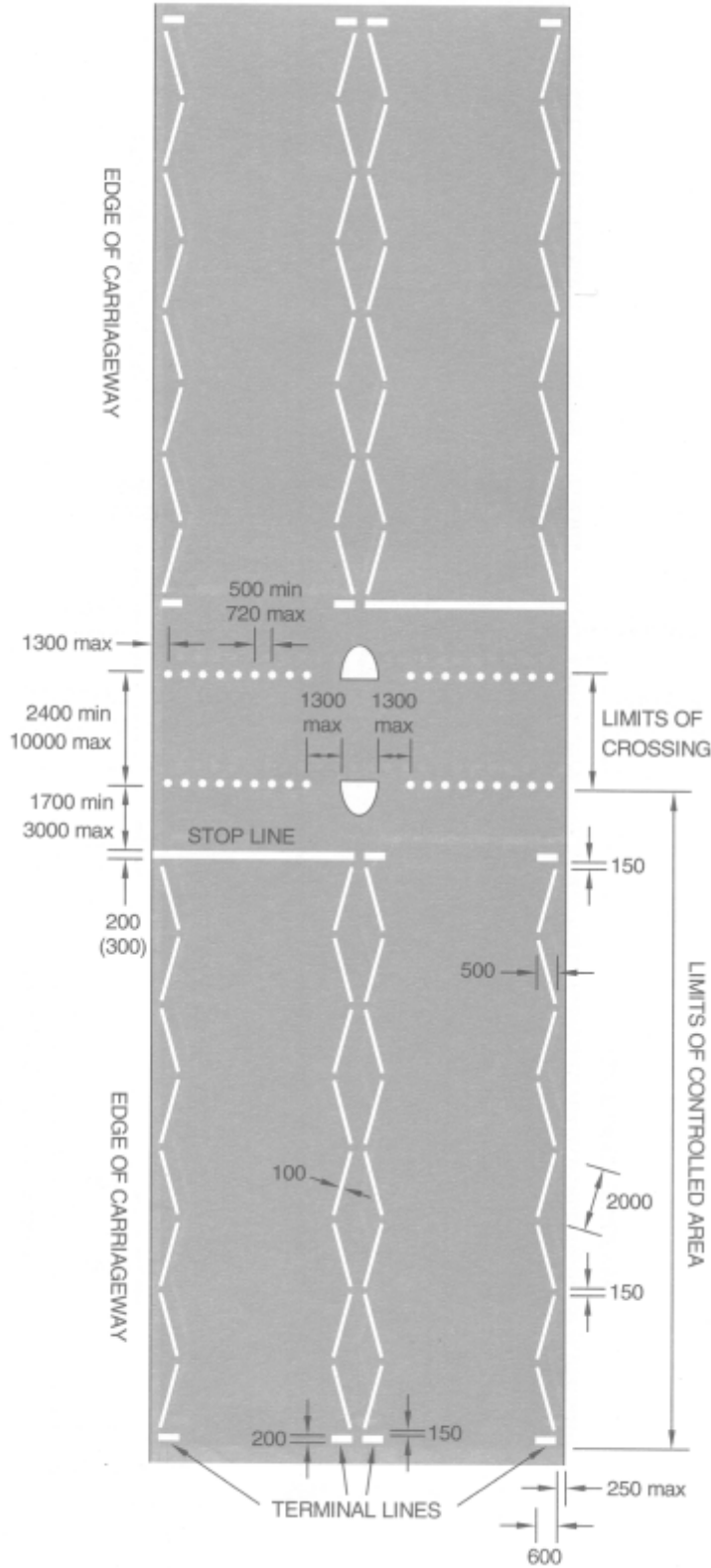


DIAGRAM 2



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DIAGRAM 3

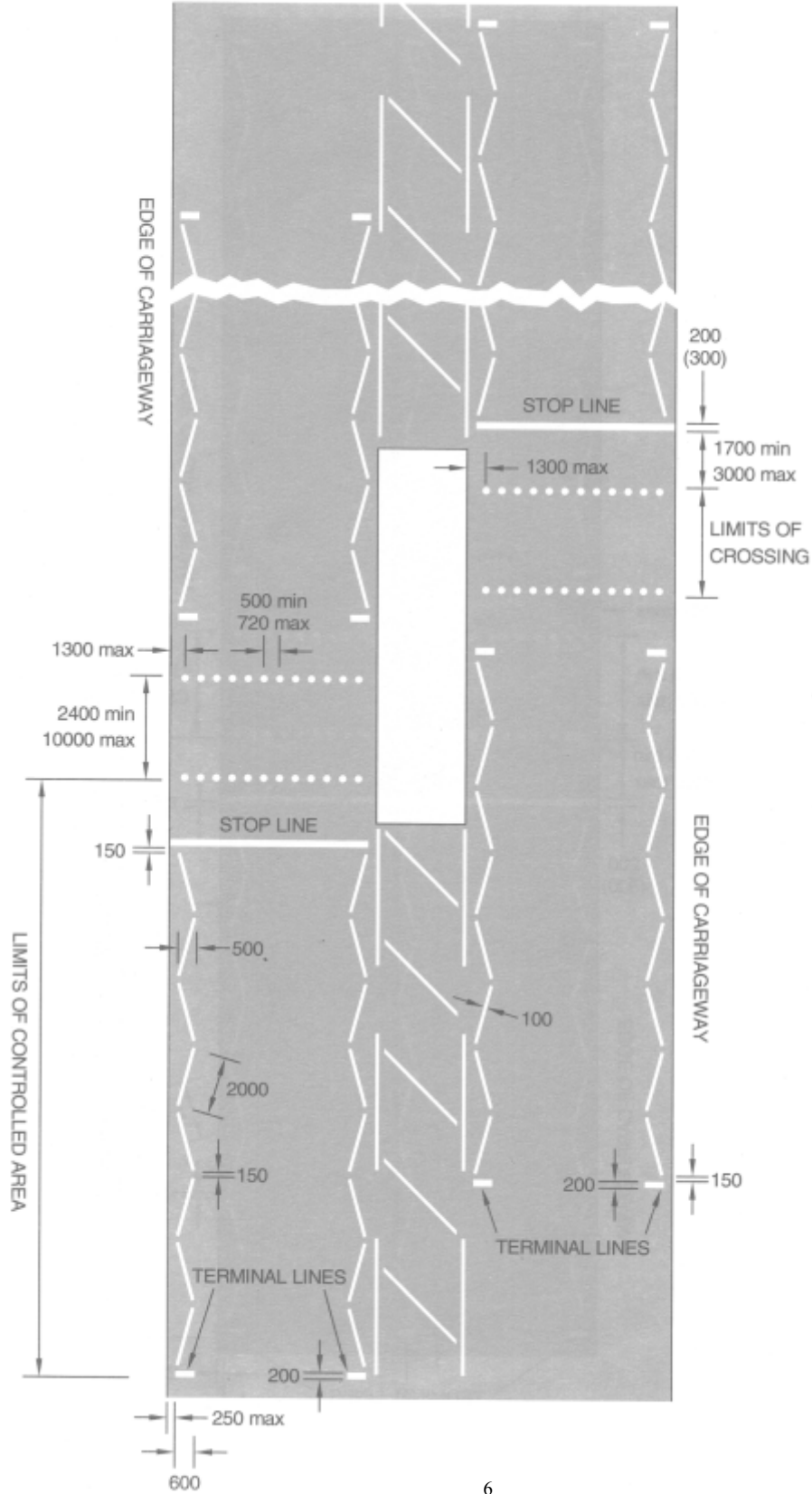
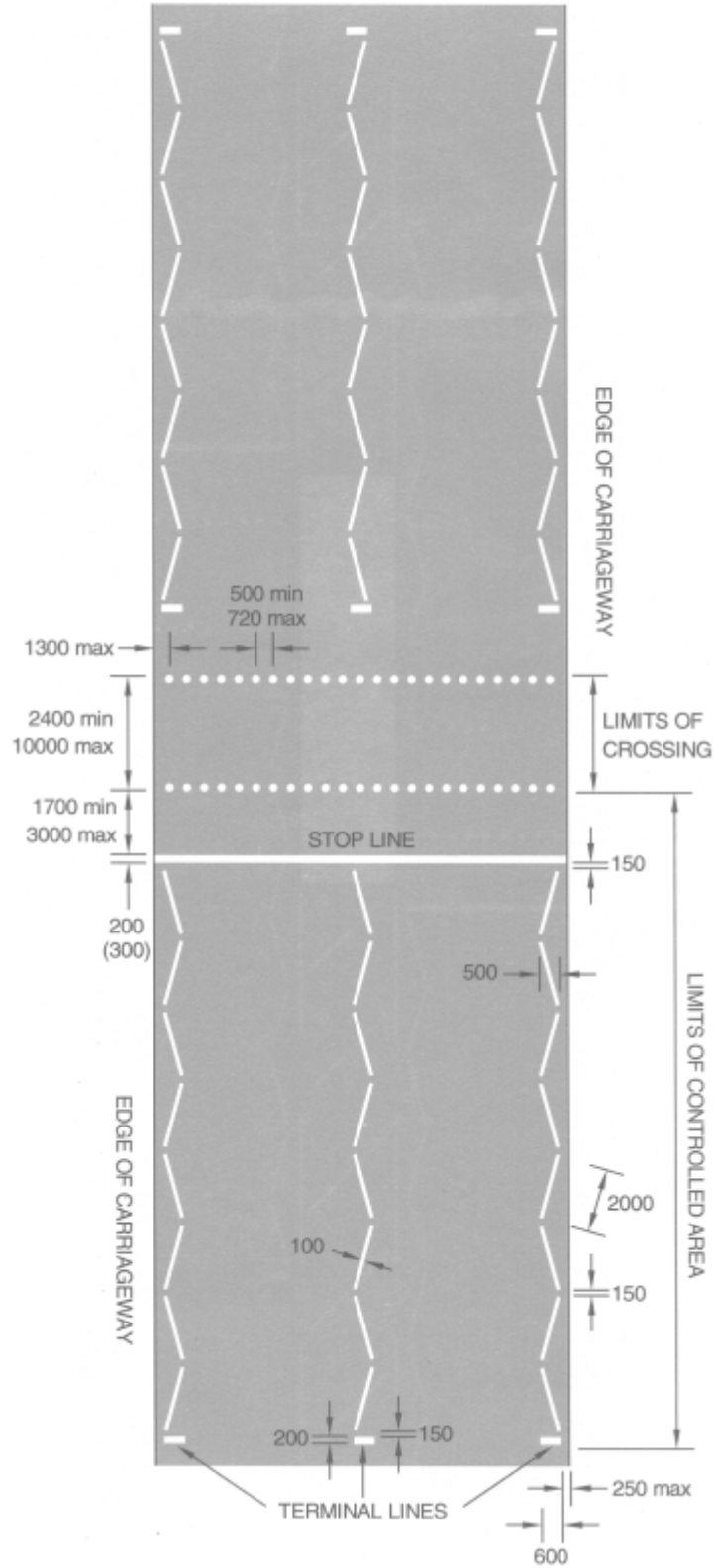
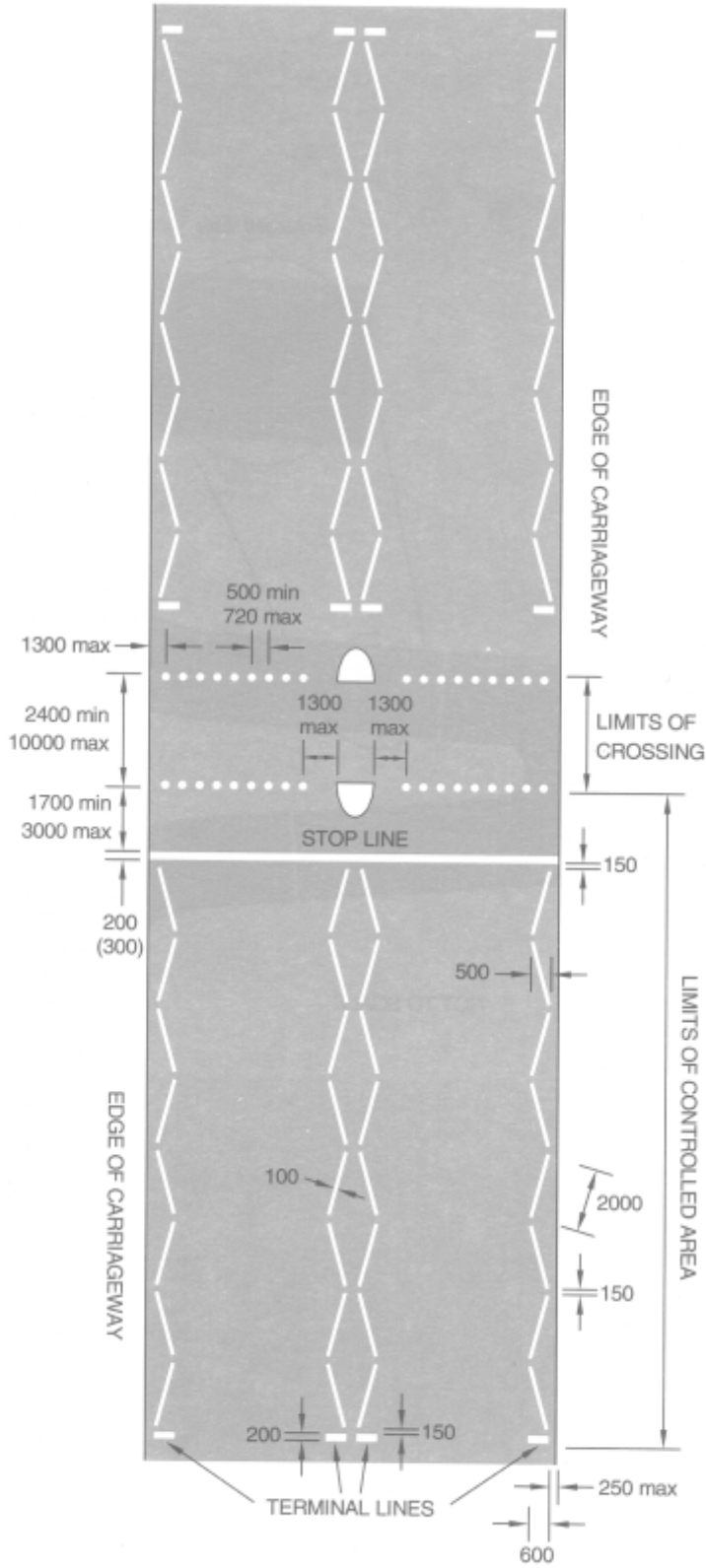


DIAGRAM 4



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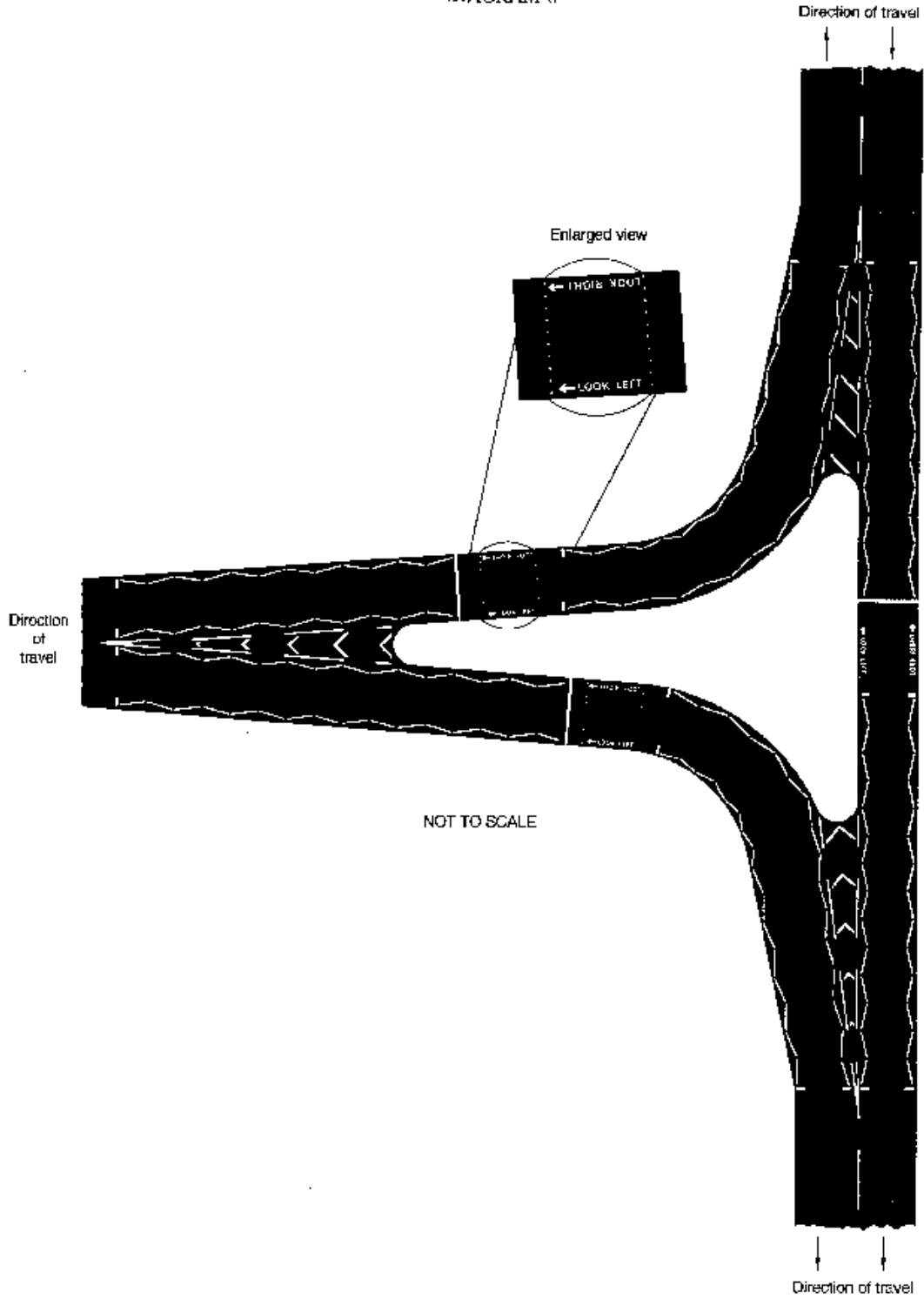
DIAGRAM 5





# DIAGRAM 6

## DIAGRAM 6



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## PART II

### THE PELICAN AND PUFFIN PEDESTRIAN CROSSINGS GENERAL DIRECTIONS 1997

#### **Citation and commencement**

1. This Part of this Instrument—
  - (a) may be cited—
    - (i) as the Pelican and Puffin Pedestrian Crossings General Directions 1997, and
    - (ii) together with Part I of this Instrument, as the Zebra, Pelican and Puffin Pedestrian Crossings Regulations and General Directions 1997; and
  - (b) shall come into force on 15th December 1997.

#### **Revocation**

2. So far as they consist of or comprise general directions, the “Pelican” Pedestrian Crossings Regulations and General Directions 1987 are hereby revoked.

#### **Interpretation**

- 3.—(1) In these Directions—
  - (a) “the Regulations” means the Zebra, Pelican and Puffin Pedestrian Crossings Regulations 1997(1); and
  - (b) the expressions listed in paragraph (2) have the same meanings as in the Regulations.
- (2) Those expressions are—
  - “the 1994 Regulations”;
  - “carriageway”;
  - “central reservation”;
  - “indicator for pedestrians”
  - “one-way street”;
  - “pedestrian light signals”;
  - “pedestrian demand unit”;
  - “Pelican crossing”;
  - “primary signal”;
  - “Puffin crossing”;
  - “secondary signal”;
  - “system of staggered crossings”;
  - “two-way street”; and
  - “vehicular light signals”.
- (3) In these Directions a reference to a numbered paragraph is to the paragraph so numbered in the direction in which the reference occurs.

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(1) Part I of this Instrument.

**Vehicular light signals at Pelican and Puffin crossings**

4.—(1) In this direction references to the left or right hand side of a crossing are to the left or right hand side as viewed from the direction of travel of vehicular traffic approaching the crossing.

(2) Subject to paragraph (4) the vehicular light signals to be provided facing each direction of approaching traffic at a Pelican or Puffin crossing which is on a two-way street and is of a type specified in column (1) of the table below shall be as specified in relation to that type of crossing in column (2).

TABLE

<i>(1) Type of crossing on a two-way street</i>	<i>(2) Vehicular light signals facing each direction of traffic</i>
Crossing not forming part of a system of staggered crossings and not having a refuge for pedestrians or central reservation within the limits of the crossing	One primary signal on the left hand side of the crossing and one secondary signal on the right hand side of the crossing
Crossing not forming part of a system of staggered crossings and having a refuge for pedestrians or central reservation within the limits of the crossing	One primary signal on the left hand side of the crossing and one secondary signal on the right hand side or on the refuge for pedestrians or central reservation
Crossing forming part of a system of staggered crossings	One primary signal on the left hand side of the crossing and one other signal, which may be either a primary signal or a secondary signal, on the right hand side of the crossing

(3) Subject to paragraph (4) the vehicular light signals to be provided facing the direction of approaching traffic at a Pelican or Puffin crossing which is on a one-way street and is of a type specified in column (1) of the table below shall be as specified in relation to that type of crossing in column (2).

TABLE

<i>(1) Type of crossing on a one-way street</i>	<i>(2) Vehicular light signals to be provided</i>
Crossing not forming part of a system of staggered crossings and not having a refuge for pedestrians or central reservation within the limits of the crossing	One primary signal on the left hand side of the crossing and one other signal, which may be either a primary or a secondary signal, on the right hand side of the crossing
Crossing not forming part of a system of staggered crossings and having a refuge for pedestrians or central reservation within the limits of the crossing	One primary signal on each side of the crossing and one other signal which may be either a primary signal (in which case it must be on the refuge for pedestrians or the central reservation) or a secondary signal
Crossing forming part of a system of staggered crossings	One primary signal on the left hand side of the crossing and a signal, which may be either a primary or a secondary signal, on the right hand side of the crossing

(4) In addition to the signals required to be placed by paragraph (2) or (3) the traffic authority may place such other primary or secondary signals at a Pelican or Puffin crossing as it thinks fit.

### **Pedestrian light signals and indicators for pedestrians at Pelican crossings**

5.—(1) At least one pedestrian light signal and at least one indicator for pedestrians shall be placed at each end of a Pelican crossing.

(2) Each pedestrian light signal shall be so placed as to be clearly visible to any person on the other side of the carriageway who wishes to use the crossing.

(3) Where there is a central reservation in a Pelican crossing, at least one indicator for pedestrians shall be placed on the central reservation.

(4) Each indicator for pedestrians shall be so placed that the push button on it may be reached by any person wishing to press it.

### **Pedestrian demand units at Puffin crossings**

6.—(1) At least one pedestrian demand unit shall be placed at each end of a Puffin crossing.

(2) Each pedestrian demand unit shall be so placed that the push button on it may be reached by any person wishing to press it.

### **Additional traffic signs**

7. A traffic sign of the size, colour and type shown in diagram 610 or 611 in Schedule 2 to the 1994 Regulations may only be placed on a refuge for pedestrians or a central reservation within the limits of a Pelican or Puffin crossing or on a central reservation which lies between two Pelican or Puffin crossings which form part of a system of staggered crossings.

### **Colouring of containers and posts**

8.—(1) The containers of vehicular light signals at a Pelican or Puffin crossing—

- (a) shall be coloured black; and
- (b) may be mounted on a backing board,

and, if so mounted the backing board shall be coloured black and may have a white border not less than 45 mm nor more than 55 mm wide which may be made of reflective material.

(2) The containers of pedestrian light signals at a Pelican crossing shall be coloured black.

(3) Where, at a Pelican or Puffin crossing, vehicular light signals, pedestrian light signals, an indicator for pedestrians or a pedestrian demand unit is mounted on a post specially provided for the purpose, the part of the post extending above ground level shall be of a single colour, which may be grey, brown, dark green or dark blue but may have marked on it one yellow or white band not less than 140 mm nor more than 160 mm deep, the lower edge of the band being not less than 1.5 metres nor more than 1.7 metres above the level of the immediately adjacent ground.

(4) Any box attached to a post or other structure on which vehicular light signals, pedestrian light signals, an indicator for pedestrians or a pedestrian demand unit are mounted and housing apparatus designed to control, or to monitor, the operation of the signals or unit shall be coloured grey, black, brown, dark green or dark blue.

### **Approval of equipment**

9.—(1) In this direction “equipment” means all equipment (including the content of all instructions stored in, or executable by it) capable of giving visible, audible or tactile signals used in connection with vehicular light signals, pedestrian light signals, indicators for pedestrians or pedestrian demand units to secure that those signals, indicators or units comply with the relevant provisions of the Regulations.

(2) All equipment placed on a road for the purposes of a Pelican or Puffin crossing shall be of a type approved in writing by the Secretary of State.

(3) If, after equipment has been placed in accordance with an approval under paragraph (2), the signals, indicator or unit used in connection with it is altered, the signal, indicator or unit shall not be further used unless that alteration is approved in writing by the Secretary of State.

(4) When any equipment which has been placed at a Pelican or Puffin crossing is of a type approved under paragraph (2), the equipment shall, subject to paragraph (3), be regarded as continuing to be approved until notice is given in writing by the Secretary of State—

(a) to the traffic authority; and

(b) either—

(i) to the supplier of the equipment; or

(ii) where an alteration has been approved in accordance with paragraph (3), to the person who carried out the alteration,

of a date which the equipment is no longer to be so regarded.

(5) Where notice is given under paragraph (4) that equipment is no longer to be regarded as being approved, the equipment and, unless the Secretary of State approves any alternative equipment for the same purpose, any signal, indicator or unit in connection with it shall be removed from the road on or before the date given in the notice.

#### **Special directions by the Secretary of State**

**10.** Nothing in these Directions shall be taken to limit the power of the Secretary of State by any special direction to dispense with, add to or modify any of the requirements of these Directions in their application to any particular case.

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## APPENDIX

### EXERCISE OF POWERS

1. The Regulations in Part I of this Instrument are made by the Secretary of State for Transport, the Secretary of State for Scotland and the Secretary of State for Wales—

- (a) acting jointly, in exercise of the powers conferred on them by section 64(1), (2) and (3) of the Road Traffic Regulation Act 1984<sup>(2)</sup>; and
- (b) acting severally, in relation to England, to Scotland and to Wales respectively, in exercise of the powers conferred on them by section 25 of that Act

and of all other powers enabling them in that behalf.

2. The Directions in Part II of this Instrument are given by the said Secretaries of State acting jointly, in exercise of the powers conferred on them by sections 65(1) and (1A)<sup>(3)</sup> of the Road Traffic Regulation Act 1984.

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(2) 1984 c. 27. For the meaning of “the Ministers” in sections 64(1)(a) and 65(1), see section 142(1).

(3) Section 65(1) was substituted by the New Roads and Street Works Act 1991 (c. 22), Schedule 8, paragraph 28 and section 65(1A) was inserted by the Road Traffic Regulation Act 1991 (c. 40), Schedule 8, paragraph 22.